

CONNECTICUT RESOURCES RECOVERY AUTHORITY

MAXIM ROAD RAILROAD CROSSING TRACK AND SIGNAL REPAIRS

MAXIM ROAD
HARTFORD, CONNECTICUT

CRRA CONTRACT NO. 080121

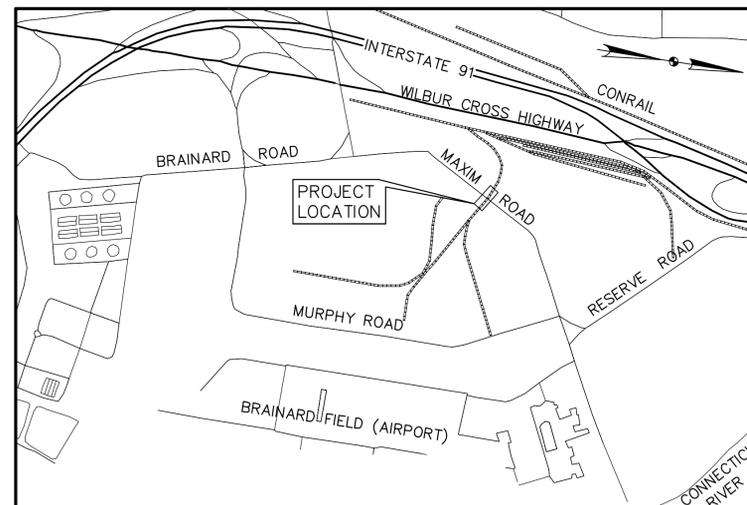
APRIL, 2010

URS
URS CORPORATION AES
500 ENTERPRISE DRIVE
ROCKY HILL, CT. 06067
1-(860)-529-8882

URS PROJECT NO. 36938181

LIST OF DRAWINGS

T-001	COVER SHEET
C-1.0	SITE PLAN
C-2.0	PROFILES
C-3.0	TYPICAL CROSS SECTIONS



LOCATION PLAN
NOT TO SCALE

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REVISIONS

NUMBER	DATE	REMARKS
1	11/20/09	CRRA REVIEW

**MAXIM ROAD
CROSSING
RECONSTRUCTION**

HARTFORD, CT.

PROJECT NO: 36938181

DRAWN BY: BNS

CHECKED BY: GRN

DATE: 11/02/09

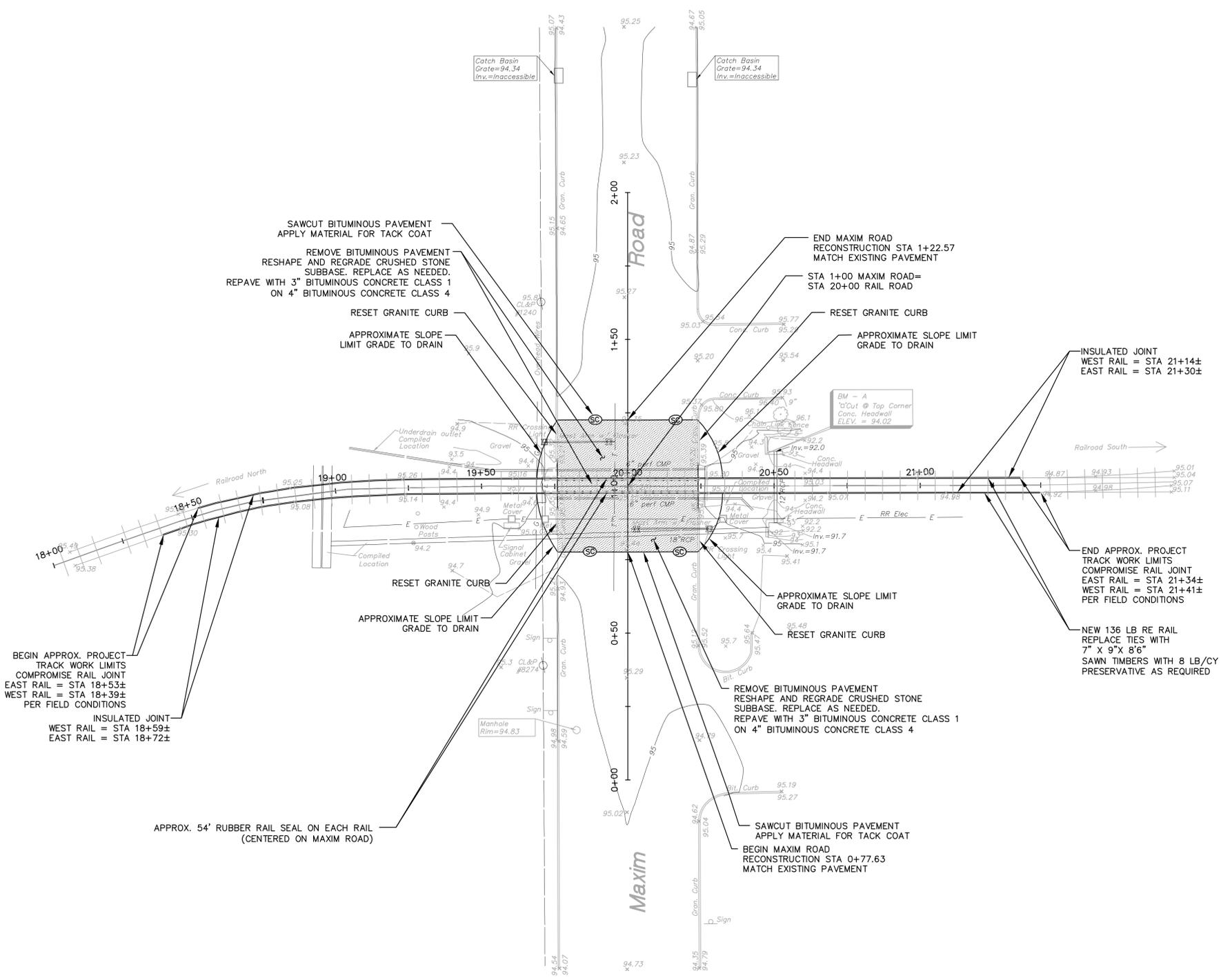
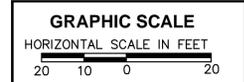
SCALE: 1" = 20'

SITE PLAN

C-1.0

NOTES

- CONTRACTOR SHALL VERIFY EXISTING FIELD CONDITIONS PRIOR TO COMMENCING WORK. NOTIFY ENGINEER OF ANY DISCREPANCIES BETWEEN EXISTING CONDITION AND CONTRACT DOCUMENTS.
- EXISTING SURFACE CONDITION IS FROM MAP ENTITLED "TOPOGRAPHIC SURVEY RAILROAD CROSSING, MAXIM ROAD, HARTFORD, CONNECTICUT" BY URS CORPORATION AES DATED OCTOBER 2009.
- DIRECTIONS OF NORTH SHOWN AS APPROXIMATE.
- ELEVATIONS REFER TO THE DATUM AS ESTABLISHED BY THE PLAN IN NOTE 4.
- REFERENCE IS MADE TO A PLAN ENTITLED "MAXIM ROAD CROSSING RENEWAL PLAN AND PROFILE", BY A.J. BELLIVEAU RAILROAD CONSTRUCTION CO. INC., DATED 10/95, REVISED 2/96.
- LOCATION OF UTILITIES SHOWN ON THIS SHEET, WITH THE EXCEPTION OF OVERHEAD UTILITIES, HAVE BEEN COMPILED FROM A PLAN ENTITLED "MAXIM ROAD CROSSING RENEWAL PLAN AND PROFILE BY A.J. BELLIVEAU RAILROAD CONSTRUCTION CO. INC., DATED 10/95, REVISED 2/96.
- CONTRACTOR SHALL REQUEST MARK OUT OF UTILITY LOCATION FROM CALL BEFORE YOU DIG (1-800-922-4455) AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION OPERATION.
- AREAS OUTSIDE THE WORK LIMIT LINE DISTURBED BY CONSTRUCTION SHALL BE RETURNED TO THEIR ORIGINAL CONDITION OR BETTER AND SHALL BE GRADED TO MEET PROPOSED CONSTRUCTION. COST FOR THIS WORK SHALL BE BOURNE BY THE CONTRACTOR.
- THE CONTRACTOR SHALL PROCURE ALL THE NECESSARY PERMITS AND LICENSES AT THE TIME REQUIRED, PAY ALL CHARGES AND FEES, AND GIVE NOTICES NECESSARY AND DUE IN CONNECTION WITH THE LAWFUL EXECUTION OF THE WORK AT NO ADDITIONAL COST TO THE OWNER.
- EXCAVATION OF ANY TYPE SHALL BE ACCOMPLISHED IN SUCH A MANNER THAT UNDERGROUND UTILITIES OR STRUCTURES ARE NOT DAMAGED. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY FOR ANY DAMAGE INCURRED DURING EXCAVATION OPERATIONS. ALL EXCAVATION SHALL BE IN CONFORMANCE WITH THE LATEST OSHA REQUIREMENTS.
- ALL UTILITY BOXES, FRAMES, GRATES, ETC. AFFECTED BY THE CONSTRUCTION ACTIVITIES, SHALL BE RESET/RECONSTRUCTED TO THE PROPER GRADE. ALL COST RELATED TO SUCH WORK SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID.
- THE RAILROAD WORK UNDER THIS PROJECT SHALL BE DONE AS SHOWN ON THE PLANS AND IN CONFORMANCE TO THE TECHNICAL SPECIFICATIONS.
- THE STANDARD SPECIFICATIONS (FOR SITE WORK) SHALL MEAN THE STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND INCIDENTAL CONSTRUCTION, FORM 816 2004, INCLUDING ALL SUPPLEMENTS THERETO. THESE SPECIFICATIONS ARE AVAILABLE FOR VIEWING ON THE CDOT WEBSITE WWW.CT.GOV/DOIT/SITE UNDER THE "PUBLICATIONS" TAB.
- LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES INSTALLED OR FOUND DURING CONSTRUCTION SHALL BE INDICATED BY THE CONTRACTOR ON RECORD DRAWINGS, MEASURED FROM PERMANENT SURFACE FEATURES.
- THE EXISTING UTILITIES WILL BE IN CONTINUOUS OPERATION DURING THE EXECUTION OF THE WORK. THE CONTRACTOR SHALL COORDINATE HIS ACTIVITIES TO MINIMIZE DISRUPTIONS TO THE UTILITIES. THE CONTRACTOR SHALL COORDINATE ALL WORK AND WORK PROCEDURES WITH THE REQUIREMENTS OF THE LOCAL AUTHORITIES. BEFORE ANY WORK PROCEEDS, NOTIFY THE OWNER'S REPRESENTATIVE OF THE PROPOSED SCHEDULE. DISRUPTION OF UTILITY OPERATIONS SHALL BE MINIMAL AND THE SAFETY OF EMPLOYEES AND THE PUBLIC SHALL BE MAINTAINED.
- A THREE DAY ROAD CLOSURE (SATURDAY, SUNDAY & MONDAY) WILL BE ALLOWED. THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT WORK WITHIN MAXIM ROAD WILL BE COMPLETED AND THE ROADWAY OPEN TO NORMAL TRAFFIC OPERATION WITHIN THE ALLOWABLE CLOSURE PERIOD.
- THE CONTRACTOR SHALL PERFORM ALL OPERATIONS OF DEMOLITION AND REMOVAL AS MAY BE REQUIRED FOR HIS WORK.
- THE CONTRACTOR SHALL COMPLETE ALL WORK SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE OR WHICH ARE TO REMAIN THE PROPERTY OF THE OWNER, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN, OR WHICH ARE TO REMAIN THE PROPERTY OF THE OWNER, THE DAMAGED MATERIALS SHALL BE REPLACED TO THE SATISFACTION OF THE OWNER'S REPRESENTATIVE AT THE EXPENSE OF THE CONTRACTOR.
- NO MATERIAL SHALL BE STORED ON SITE UNLESS THE PROPOSED STORAGE LOCATION IS APPROVED BY THE OWNER'S REPRESENTATIVE.
- AFTER THE CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL REMOVE ALL EXCESS MATERIAL, DEBRIS, ETC. AND RESTORE OR REPAIR ANY DAMAGE TO LANDSCAPING AS DIRECTED BY THE OWNER'S REPRESENTATIVE.
- ALL PROPOSED WORK MAY BE VARIED IN THE FIELD BY THE OWNER'S REPRESENTATIVE TO MEET EXISTING CONDITIONS.
- REMOVE EXISTING RAIL AND TIES WITHIN THE PROJECT LIMITS. MATERIAL REMOVED SHALL BE DISPOSED OFF SITE AT THE EXPENSE OF THE CONTRACTOR.



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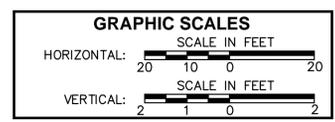
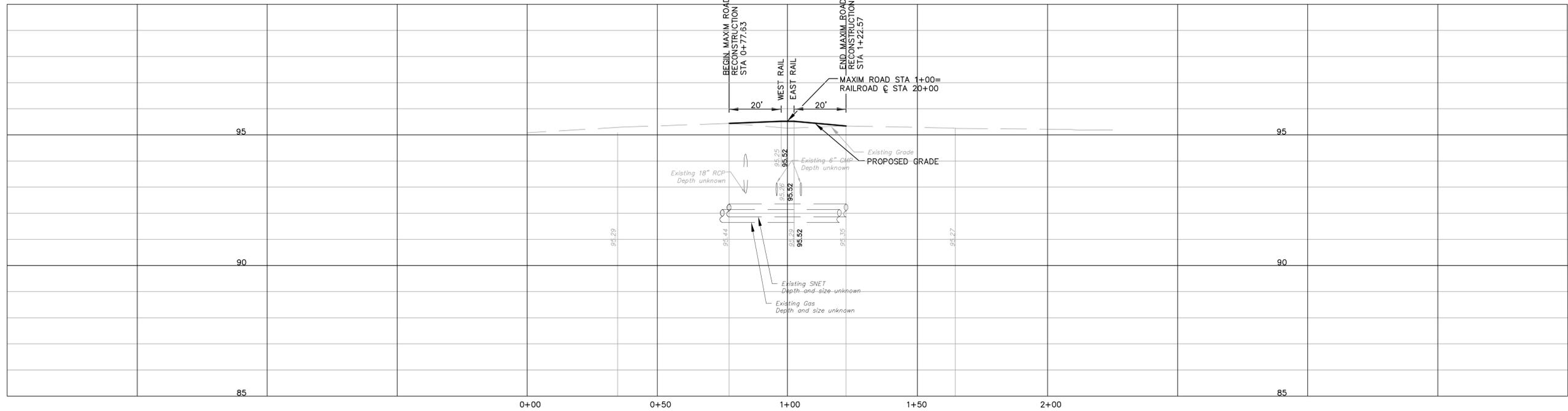
**MAXIM ROAD
CROSSING
RECONSTRUCTION**

HARTFORD, CT.

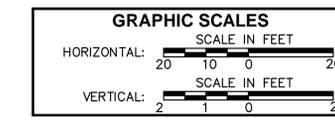
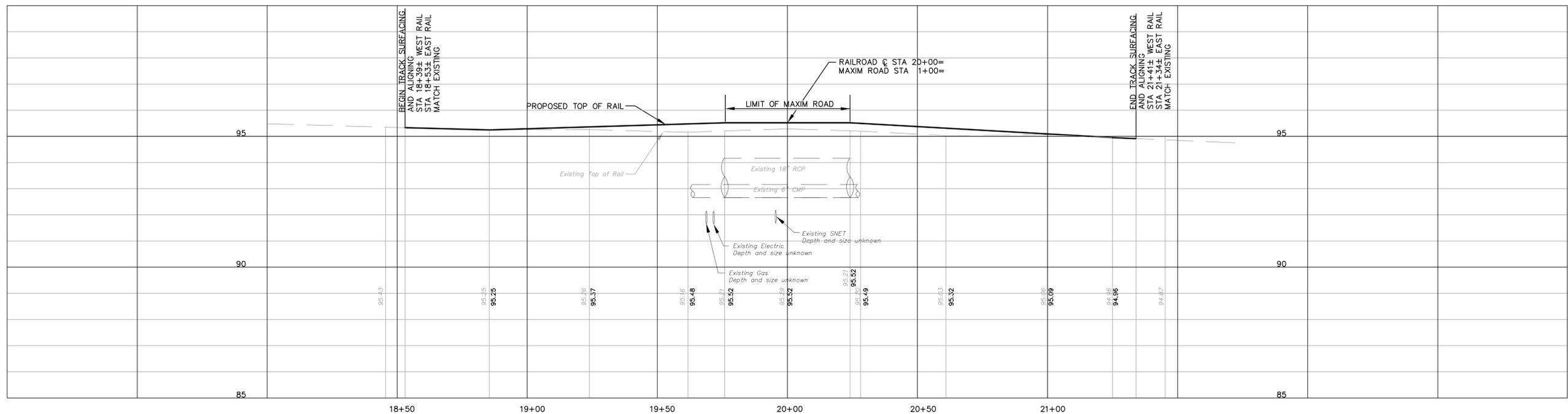
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SCALE	AS NOTED

PROFILES

C-2.0



**PROFILE ALONG
MAXIM ROAD**



**PROFILE ALONG
EAST RAIL**

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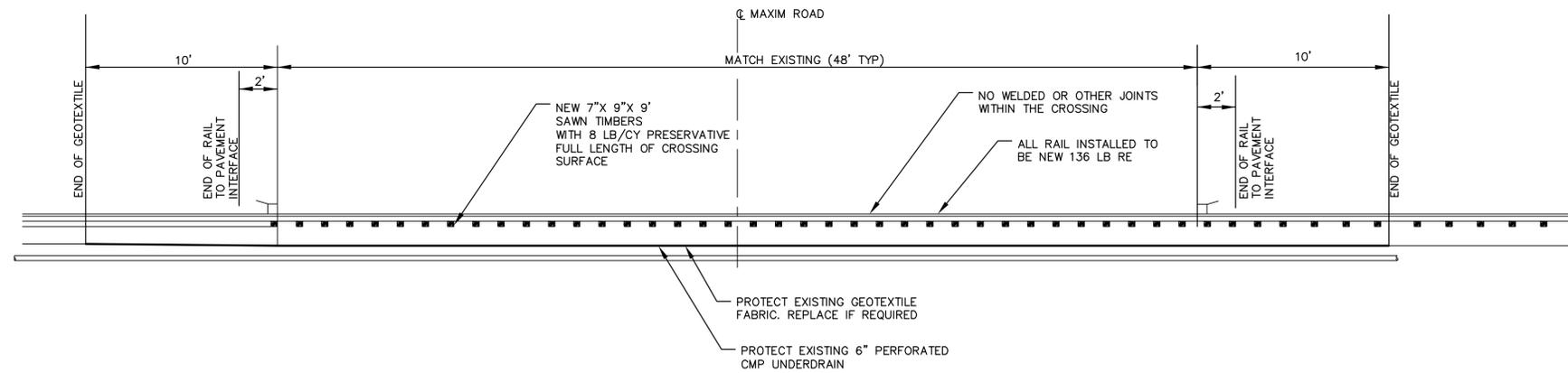
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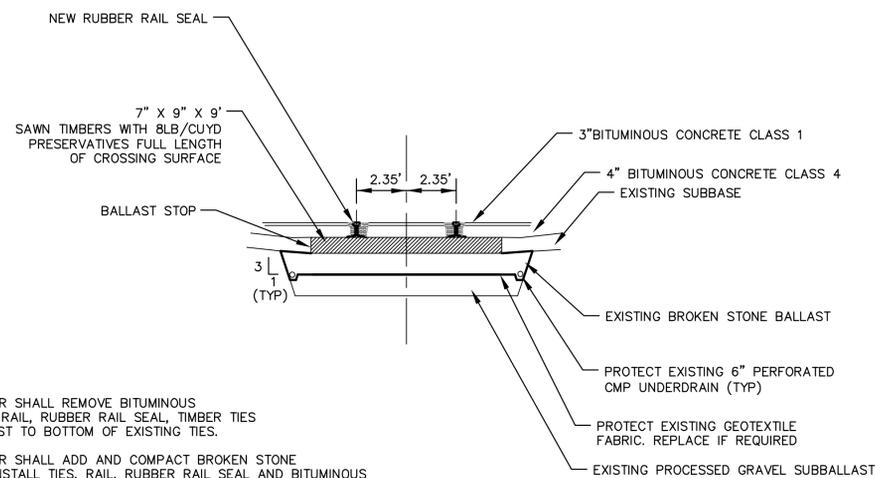
**TYPICAL
CROSS SECTIONS**

C-3.0

NOTE
WELDED 136 RE RAIL TO EXTEND A MINIMUM OF 20' PAST
INSULATED JOINTS ON BOTH SIDES OF CROSSING



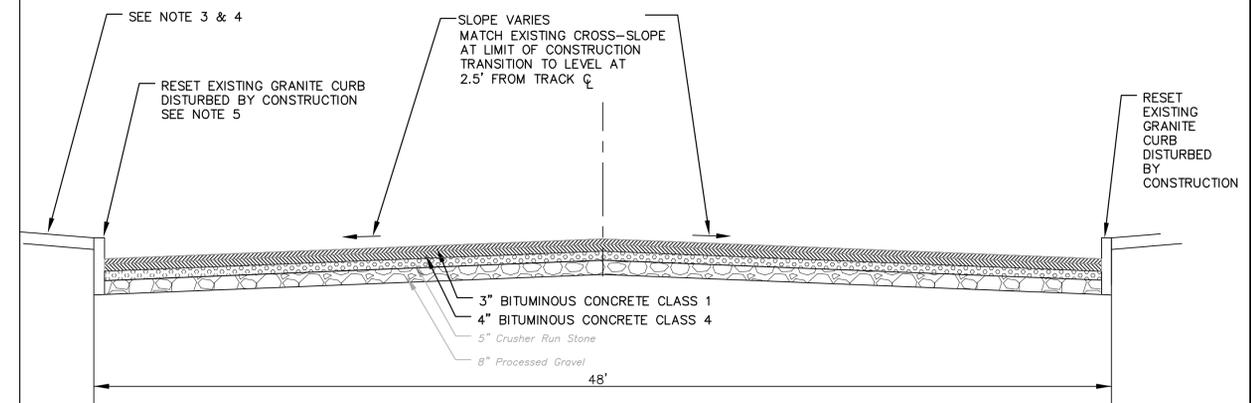
**SECTION THROUGH TRACK AT
CENTERLINE OF MAXIM ROAD**
SCALE: 1" = 4'



NOTES

- CONTRACTOR SHALL REMOVE BITUMINOUS PAVEMENT, RAIL, RUBBER RAIL SEAL, TIMBER TIES AND BALLAST TO BOTTOM OF EXISTING TIES.
- CONTRACTOR SHALL ADD AND COMPACT BROKEN STONE BALLAST, INSTALL TIES, RAIL, RUBBER RAIL SEAL AND BITUMINOUS PAVEMENT TO BRING CROSSING TO THE GRADE SHOWN ON THE PLAN.

**CROSS SECTION OF
CROSSING SURFACE AND
RELATED ITEMS**
SCALE: 1" = 4'



NOTES

- REMOVE BITUMINOUS MATERIAL FROM PORTION OF MAXIM ROAD SHOWN ON PLANS.
- RESHAPE AND REGRADE CRUSHER RUN STONE LAYER TO ACCEPT NEW BITUMINOUS PAVEMENT.
- BACKFILL BEHIND RESET CURB WITH TOP SOIL AND FERTILIZE, SEED AND MULCH OR WITH CRUSHED STONE TO MATCH EXISTING.
- GRADE AREAS BEYOND CURB TO DRAIN.
- GRANITE CURB SHALL HAVE 6" REVEAL TO WITHIN 12 FT OF THE TRACK AND TAPER TO 0" REVEAL 6 FT FROM TRACK.

**TYPICAL SECTION
MAXIM ROAD**
SCALE: 1" = 4'